

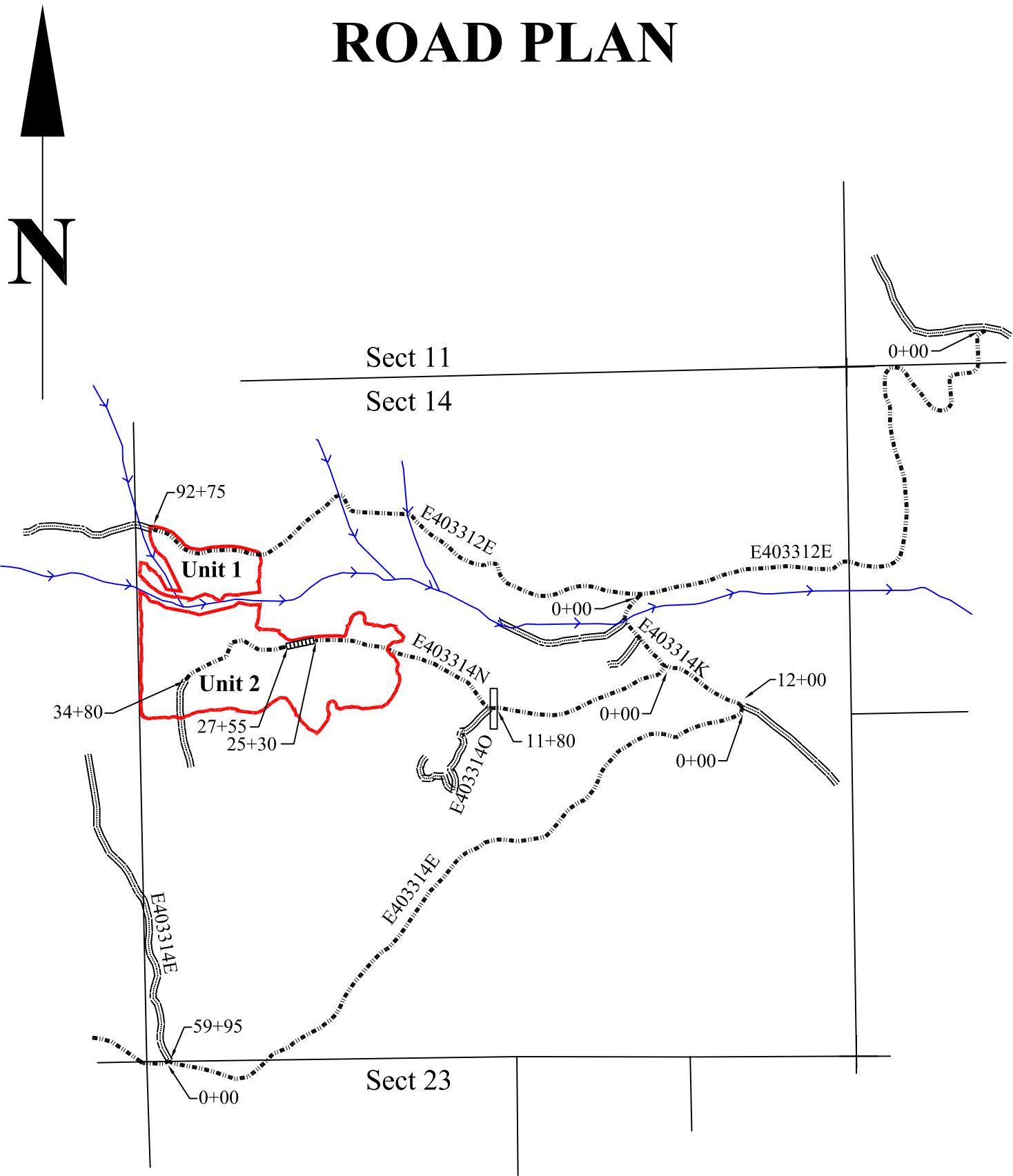
STATE OF WASHINGTON

DEPARTMENT OF NATURAL RESOURCES

Agreement No.: 30-078598
Name of Sale: Bull Goose
Trust: 03

Region: Northeast
County: Ferry

TOWNSHIP 40 NORTH, RANGE 33 EAST, W. M.



LEGEND

Sale Area Boundary Line	<div></div>
Existing Road	<div></div>
Prehaul Maintenance	<div></div>
New Construction	<div></div>
Reconstruction	<div></div>
Install Culvert	<div></div>

Drawn by: Jason Bauer
Date: 11/28/05

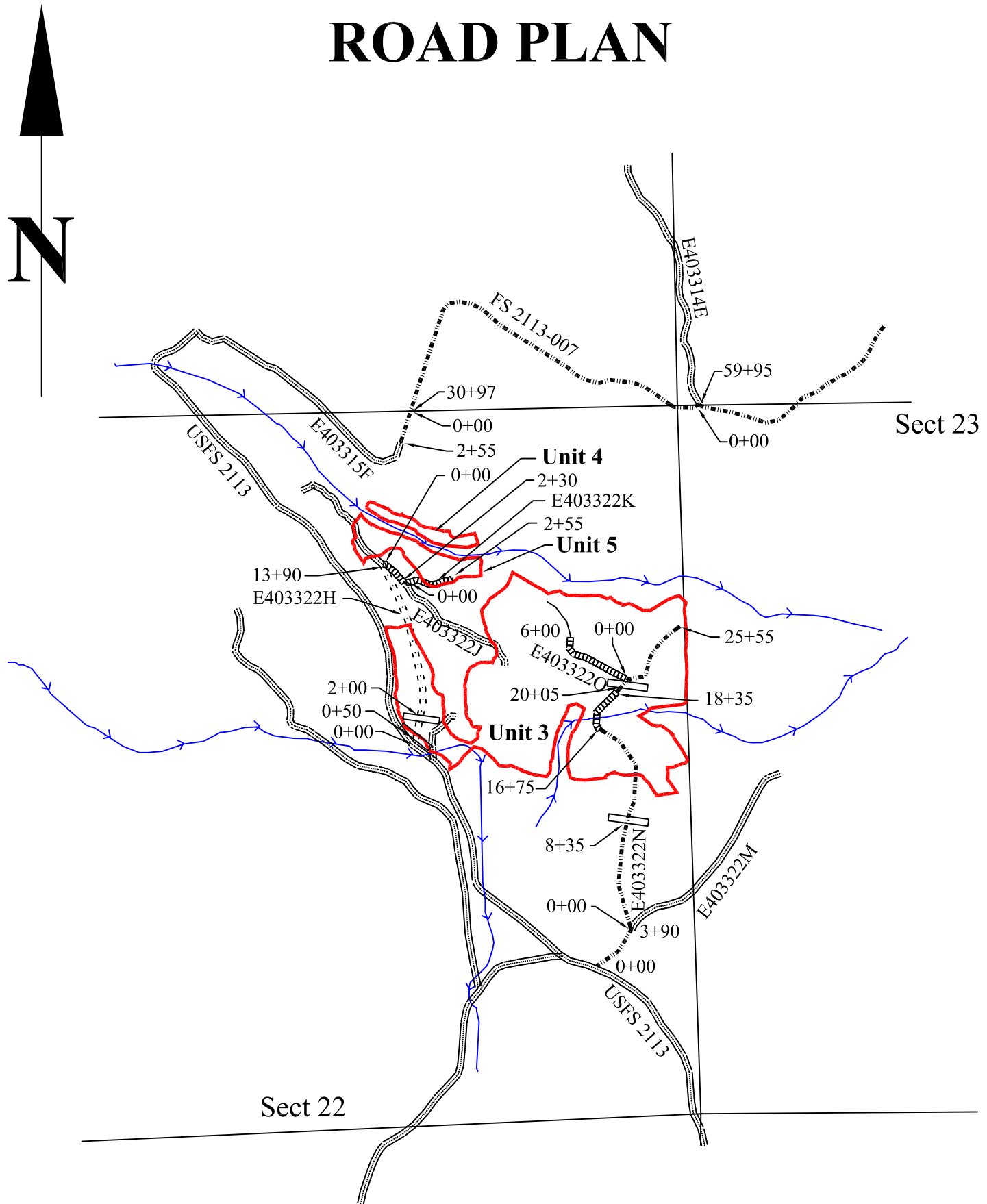
DEPARTMENT OF NATURAL RESOURCES

Agreement No.: 30-078598
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ROAD PLAN



LEGEND

Sale Area Boundary Line	
Existing Road	
Prehaul Maintenance	
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Install Culvert	

Drawn by: Jason Bauer
Date: 11/28/05

STATE OF WASHINGTON
DEPARTMENT OF NATURAL RESOURCES

ROAD PLAN

AGREEMENT NUMBER: 30-078598

SALE NAME: Bull Goose

ROAD PLAN DATE: 11-28-05

SCOPE OF PROJECT

This project includes, but is not limited to construction including; clearing; grubbing; right-of-way debris disposal; excavation and/or embankment to subgrade, acquisition and installation of drainage structures, drilling and blasting of rock in the course of right-of-way construction may be encountered.

DEFINITIONS

Construction

Where in the terms of this contract the activity of building a new right of way and road over ground that has not had a previously established road, or is a relocation that is at least a full right of way width from an existing road.

Reconstruction

Where in the terms of this contract the activity of right of way development and road building in a location that lies completely or partially within the right of way of an existing road and which activity is required or recommended to improve upon the pre-existing conditions. Reconstruction includes activities that would be classified as a **Class II, Class III or Class IV Special Forest Practice**.

Pre-haul Maintenance

Where in the terms of this contract the activity of right of way development and road building in a location that lies completely or partially within the right of way of an existing road and which activity is required or recommended to improve upon the pre-existing conditions. Pre-haul maintenance includes activities that would be classified as a **Class 1 Forest Practice**.

Maintenance

Where the pre-existing conditions of an existing road were acceptable to the State prior to this contract, and the repair and/or replacement of materials, components or structures become necessary as result of deterioration by use or inordinate damage during the terms of this contract.

Designated Skid Trail

Where the State has marked a preferred location for the forwarding of valuable materials to an established road or landing, and which alternatives to the designated location require approval from the Contract Administrator.

Abandonment

Where in the terms of this contract the activity of blocking, controlling the erosion and water movement within natural drainages, removing all drainage structures, and removing unstable fill slopes that have delivery potential. This activity may include but is not limited to obliteration of road prisms, haying and revegetation of exposed soils, and scattering of natural debris.

SECTION 1 - GENERAL CLAUSES

- 1.1
- Clauses in this plan apply to all construction and/or reconstruction, including landings unless otherwise noted.
- 1.2
- Construction, reconstruction, pre-haul maintenance, and abandonment of the following road/s are required. All road/s shall be constructed on the State's location, and in accordance with this Road Plan.

<u>Road</u>	<u>Length</u>	<u>Type</u>
USFS 2113-007	30.97 stations	Pre-Haul Maintenance
403312E	92.75 stations	Pre-Haul Maintenance
403314E	59.95 stations	Pre-Haul Maintenance
403314K	12.00 stations	Pre-Haul Maintenance
403314N	32.55 stations	Pre-Haul Maintenance
403314N	2.25 stations	Reconstruction
403315F	2.55 stations	Pre-Haul Maintenance
403322H	13.90 stations	Construction
403322J	2.30 stations	Reconstruction
403322K	2.55 stations	Reconstruction
403322M	3.90 stations	Pre-Haul Maintenance
403322N	23.95 stations	Pre-Haul Maintenance
403322N	1.60 stations	Reconstruction
403322O	6.00 stations	Reconstruction

- 1.4
- This sale also includes, but is not limited to reconstruction including;

<u>ROAD</u>	<u>STATIONS</u>	<u>REQUIREMENTS</u>
403314N	25+30 to 27+55	Rebuild cut slopes, and subgrade, brush, and install drainage structures.
403322J	0+00 to 2+30	Widen curves, rebuild subgrade, brush, and install drainage structures.
403322K	0+00 to 2+55	Widen curves, rebuild subgrade, brush, and install drainage structures.
403322N	16+75 to 18+35	Widen curves, rebuild subgrade, brush, and install drainage structures.
403322O	0+00 to 6+00	Widen curves, rebuild subgrade, brush, and install drainage structures.

1.5 This sale also includes, but is not limited to pre-haul maintenance including;

<u>ROAD</u>	<u>STATIONS</u>	<u>REQUIREMENTS</u>
USFS 2113-007	0+00 to 30+97	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403312E	0+00 to 92+75	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403314E	0+00 to 59+95	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403314K	0+00 to 12+00	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403314N	0+00 to 25+30	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403314N	27+55 to 34+80	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403315F	0+00 to 2+55	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403322M	0+00 to 3+90	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403322N	0+00 to 16+75	Brush right of way, clear, blade and shape road surface, and add rolling dips.
403322N	18+35 to 25+55	Brush right of way, clear, blade and shape road surface, and add rolling dips.

- 1.6 If the Purchaser desires a road location or design change, a revised Road Plan shall be submitted to the State for consideration.
- 1.7 On this plan, quantities are minimum acceptable values. Additional quantities required by the State because of hidden conditions, or the Purchaser's choice of construction season or techniques shall be at the Purchaser's expense. Hidden conditions include, but are not limited to, solid subsurface rock, subsurface springs or saturated ground, and/or unstable soil conditions.
- 1.8 Purchaser shall not use roads constructed or reconstructed under this Road Plan for hauling, other than timber cut on the right-of-way, without written approval from the Contract Administrator.
- 1.9 Roads shall be constructed using track mounted hydraulic or cable excavators unless otherwise authorized, in writing, by the Contract Administrator.

- 1.10 Pioneering shall not extend past construction that will be completed during the current construction season. Drainage shall be provided on all uncompleted construction, as approved in writing by the Contract Administrator.

Clearing and grubbing shall be completed prior to starting excavation and embankment.

Culvert placement in live streams shall precede embankment where culverts are to be placed along natural ground slopes. Temporary diversion culverts shall be provided when designed culverts are elevated above natural ground within embankments.

Culverts shall be installed in completed subgrade as construction progresses.

Subgrade, ditches and culvert installation shall be completed and are subject to written approval by the Contract Administrator prior to the application of rock, or final subgrade compaction.

- 1.12 Construction restrictions apply to this contract. All construction and transportation of heavy equipment and/or trucks is prohibited between and including the following dates, except as may be authorized, in writing, by the Contract Administrator.

November 1 to June 15

- 1.14 Landings shall be sloped sufficiently to provide controlled drainage, without ponding or concentration of sediments into streams.

- 1.21 Maintenance on all road/s used, constructed or reconstructed under this Road Plan shall be performed in accordance with the Forest Access Road Maintenance Specifications.

Rutting of finished road surfaces shall not exceed 4 inches in depth. In the event that surface or base stability problems may persist, the purchaser/contractor will be required to cease operations, or perform corrective maintenance and/or repairs, subject to specifications within this contract, and the written approval of the Contract Administrator.

- 1.23 Snowplowing shall not be permitted unless authorized, in writing, by the Contract Administrator.

SECTION 2 - CLEARING

- 2.1 Fell all vegetative material larger than 6 inches d.b.h., or over 20 feet high between the marked right of way boundaries, and within waste and/or debris areas. If clearing limits are not marked in the field, clearing limits are as specified on the Typical Section Sheet.

- 2.2 Deck all merchantable right-of-way timber. The decks shall be parallel to the road centerline, and within the cleared right-of-way. The decks shall be free of dirt, limbs and other debris, and removable by standard log loading equipment from the completed roadbed.

- 2.3 Brushing shall be done in accordance to the attached Brushing Detail. Brushing shall be achieved by manual or mechanical cutting of brush, trees and braches. Root systems and stumps of cut vegetation shall not be disturbed unless specified otherwise. Pulling, digging, pushing over, and other methods used for vegetation removal are not acceptable forms of brushing. The Contractor is required to submit a detailed list of equipment and methods to be used during brushing for approval by the Contract Administrator prior to commencing work.

SECTION 3 - GRUBBING

- 3.1 All stumps shall be removed that fall between grubbing limits shown on the Typical Section Sheet. Those with undercut roots shall be removed. Stumps over 22 inches in diameter shall be split. Stumps over 40 inches in diameter shall be quartered.
- 3.2 Grubbing Limits are defined as the entire area between the external limits shown on the Typical Section Sheet.
- 3.3 Removal of stumps shall not be required, within the waste and/or debris areas, provided that they are cut flush with the ground.

SECTION 4 - DEBRIS DISPOSAL AND REMOVAL

- 4.1 Right-of-way debris is defined as all nonmerchantable vegetative material larger than one cubic foot in volume, within waste area and/or clearing limits as shown on the Typical Section Sheet.
- 4.2 Right-of-way debris shall be piled. Debris piles shall be made to be burnable, clean, tight, and free of rock or soil material. Piles shall be made no closer than 20 feet to standing timber, and no higher than 10 feet. Dozer blades shall not be permitted for debris piling.
- 4.3 Debris piles shall be placed within the cleared right-of-way, or in natural openings, as designated by the Contract Administrator. Placement of debris piles outside of the right-of-way limits is subject to the written approval of the Contract Administrator.

SECTION 5 - EXCAVATION

- 5.1 Unless controlled by construction stakes or specific design sheets herein, road/s shall be constructed in accordance with dimensions shown on the Typical Section Sheet.

Excavation and embankment slopes shall be constructed to a uniform line, and left rough for easier vegetation.

Organic material shall be excluded from road prism embankments.

Road pioneering operations shall not undercut the final slope, deposit excavated material outside the clearing limits, or restrict drainage.

- 5.2 Purchaser or road construction contractor shall not bury merchantable material.

5.3 The construction of road grade and alignment shall conform to the State's marked location. The reconstruction of existing road grades shall conform to the original location except as directed by the contract administrator. Grade and alignment shall have smooth continuity, without abrupt changes in direction.

Construction limitations are as follows:

<u>Favorable Grade</u>	<u>Adverse Grade</u>	<u>Minimum Curve Radius</u>
18%	12%	60 feet

Changes in road grade shall not exceed 7%, except as required in this clause.

Adverse grades on curves shall not exceed 10 percent of the curve radius.

Favorable grades through switchbacks shall not exceed 12%.

Transition grades entering and leaving switchbacks shall not exceed a 5% grade change.

The switchback is defined as, the curved segment of road, between a beginning and end of the same curve, where the change of traffic travel direction is greater than 90 degrees.

Transition grades required to meet switchback grade limitations, shall be constructed on the tangents preceding and departing from the switchbacks.

5.5 Curve widening shall be added to the inside of curves as follows:

2 feet extra	80 to 100 foot radius curves
4 feet extra	60 to 80 foot radius curves

5.7 Roads shall be built to the dimensions shown on the Typical Section Sheet.

5.8 Except as construction staked or designed, excavation slopes shall be constructed no steeper than shown on the following table:

<u>Material Type</u>	<u>Excavation Slope Ratio</u>
Common Earth (on side slopes to 55%)	1:1
Common Earth (55% to 70% side slopes)	3/4:1
Common Earth (on slopes over 70%)	1/2:1
Fractured or loose rock	1/2:1
Hardpan or solid rock	1/4:1

5.9 Excavation and embankment slopes shall be constructed to a uniform line and left rough for easier re-vegetation.

5.10 Except as construction staked or designed, each embankment side shall be widened as follows:

<u>Height at Centerline</u>	<u>Subgrade Widening</u>
Less than 6 feet	2 feet
6 feet and over	4 feet

- 5.11 Except as construction staked or designed, embankment slopes shall be constructed no steeper than shown on the following table:

<u>Material Type</u>	<u>Embankment Slope Ratio</u>
Common earth and rounded gravel	1-1/2:1
Angular rock	1-1/4:1
Sandy soils	2:1

- 5.12 All embankment and waste material shall be compacted. The minimum acceptable compaction is achieved by placing embankments in 2 foot or shallower lifts, and routing excavation equipment over the entire width of each lift. Except for areas specifically requiring keyed embankment construction, side hill embankments too narrow to accommodate excavation equipment may be placed by end dumping or side casting until sufficiently wide to support the equipment.
- 5.14 Except as construction staked or designed, where side slopes exceed 45 percent, full bench construction shall be utilized for the entire subgrade width.
- 5.15 Waste material may be deposited adjacent to the road prism on side slopes up to 55 percent if the waste material is compacted, free of debris, and more than 50 feet away from live streams and/or culvert installations. On side slopes of 55 percent or more, all excavation shall be endhailed or pushed to designated embankment sites, except as approved in writing by the Contract Administrator. All waste embankments shall be compacted in horizontal layers not exceeding 2 feet.
- 5.24 Turnouts shall be constructed at a maximum distance of 1000 feet apart, unless shown otherwise on drawings. Turnouts shall be intervisible.

SECTION 6 - DRAINAGE

- 6.1 Finished subgrade and running surfaces shall be sloped as shown on the Typical Section Sheet, uniform, firm, rut-free, and shaped to ensure road surface runoff in an even, unconcentrated manner.
- 6.2 Berms shall be removed from shoulders to permit water runoff. The construction of ditchouts will be required where ponding will result from the effects of sidecast debris and waste material.
- 6.4 Permanent culverts shall be installed as part of this contract. The minimum requirements shall be as designated on the Culvert and Drainage list, and the Road Plan map. Permanent culverts shall be supplied, installed, and maintained by the purchaser during the limits of this contract.
- Permanent culverts shall be galvanized (AASHTO Specification No. M36) or corrugated polyethylene tubing (AASHTO Specification No. M196) culverts as designated on the Culvert List.
- Annular corrugated bands and culvert ends shall be used on metal culverts. Bands shall have a minimum width of 12 inches. Manufacturer's approved connectors shall be used for corrugated polyethylene tubing.
- 6.5 Metal, concrete, or plastic culverts and bands removed from the roadbed shall be removed from state land prior to termination of this contract.
- 6.10 On required roads, permanent culverts, downspouts, flumes, bands and gaskets as listed on the Culvert List which are not installed, shall remain the property of the State.

6.11 Culvert, downspout, flume and energy dissipater installation shall be in accordance with the Culvert and Drainage Specification Detail.

6.13 Any damaged galvanized coating or cut ends shall be treated with a minimum of 2 coats of zinc rich paint.

6.14 Cross drains and surface culverts on road grades in excess of 3 percent shall be skewed at least 30 degrees, from perpendicular to the road centerline.

Cross drain culverts shall be installed at a slope steeper than the incoming ditch grade, but not less than 3 percent nor more than 10 percent.

6.16 Installation of culverts 30 inches in diameter and over shall be subject to written approval by the Contract Administrator prior to commencing the backfill.

Pipe arches and/or multi-plate culverts shall be installed according to the National Corrugated Pipe Association Installation Manual, and are subject to the inspection and approval of the Contract Administrator prior to placement and backfill.

6.18 Outfalls from drainage structures shall not terminate directly on unprotected soil because of the potential for erosion. Downspouts, flumes and energy dissipaters shall be installed to prevent erosion, and are subject to the approval of the Contract Administrator.

Downspouts and flumes longer than 10 feet shall be staked on both sides at maximum intervals of 10 feet with 6-foot heavy-duty steel posts, and fastened securely to the posts with No. 10 galvanized smooth wire, in accordance with the Culvert and Flume Installation Detail.

6.20 Ditch reshaping, new ditch construction, and headwall construction shall be done concurrently with construction or reconstruction of the subgrade, and prior to any application of surfacing rock. Ditches shall drain to culverts, ditchouts, and natural drainages. The shape of ditches shall be in accordance with the Typical Section Sheet and the Culvert and Drainage Specification Detail.

Site indicative ditching may be required on this project regardless of road template specifications on the Typical Section Sheet, which may require insloping or outsloping of the subgrade. The purchaser/operator will be required, as part of this contract, to construct ditches as directed by the Contract Administrator, where unforeseen site conditions dictate. Ditching will generally be required to control runoff on steeper grades, in conjunction with culvert installations, and approaches to fill embankments.

6.22 Catch basins shall be constructed to resist erosion, with back slopes consistent with standards in Section 5. Minimum dimensions shall be 4 feet wide and 4 feet long, unless specified otherwise on the Culvert List.

6.23 Headwalls shall be constructed in accordance with the Culvert and Drainage Specification Detail. Headwalls shall be constructed at all cross drain culverts, except temporary culverts. Headwalls shall also be constructed at any culvert identified on the Culvert and Drainage List that specifies the placement of riprap.

6.24 Embankment slopes adjacent to culvert inlets and/or outlets at live stream crossings shall be armored with riprap, for a distance of 1 culvert diameter on each side of the pipe, and 1 culvert diameter above the pipe in accordance with the Culvert and Drainage Specifications Detail.

- 6.28 Rolling dips shall be constructed in accordance with the Rolling Dip Detail, at a maximum spacing that will produce a vertical drop of no more than 10 feet between constructed dips, or between natural drainage paths. The spacing and number of rolling dips may be adjusted by the Contract Administrator.

Rolling dips are constructed by increasing the outslope of the subgrade surface at the required locations. This includes a gradual transition into and out of the rolling dip from the subgrade template as specified on the Typical Section Sheet.

Excavated material from rolling dip construction shall not remain on the subgrade surface in the form of a berm or waterbar. Sidecasting of material removed with excavation equipment shall be sufficient to form a ditchout to direct surface runoff into, through, and away from the rolling dip, so as to not create ponding.

Discharge of water from rolling dips should be directed to reduce sediment movement and sideslope erosion. Direct the discharge into debris concentrations, onto rocky sites, preferably onto ridges rather than directly into draws. Construction location and workmanship are subject to the approval of the Contract Administrator.

- 6.30 Riprap required as part of the contract shall conform to the minimum riprap specifications for the Riprap Type listed below. Minimum specifications require that riprap be placed at a width of one culvert diameter on each side of the culvert entrance and/or outlet, and to a height of two culvert diameters above the top of the culvert. Use of materials and other sources of riprap are subject to the written approval of the Contract Administrator.

Riprap Type

Light Loose Riprap

- 6.32 Riprap shall be set in place in conjunction with the construction of embankments. Riprap shall be placed on shoulders, slopes, around culvert inlets and/or outlets as designated on the Culvert and Drainage List or as directed by the Contract Administrator. No placement by end-dumping or dropping of riprap shall be allowed. Riprap shall not restrict the flow of water into culvert inlets or catchbasins.

- 6.33 Riprap specifications require the material to be hard, sound and durable. It shall be free from segregation, seams, cracks and other defects that tend to destroy its resistance to weather and stream action. The riprap material shall be free of rock fines, soil, organic debris, or other extraneous material.

Heavy Loose Riprap - shall meet the following grading requirements:

Min / Max	Minimum Size	Maximum Size
40% / 90%	35" (2001 lbs)	---
70% / 90%	20" (373 lbs)	---
10% / 30%	---	10" (47 lbs)

Light Loose Riprap - shall meet the following grading requirements:

Min / Max	Minimum Size	Maximum Size
20% / 90%	20" (373 lbs)	36" (2177 lbs)
80% / --	12" (81 lbs)	30" (1260 lbs)
10% / 20%	---	10" (47 lbs)

Concrete Slabs - shall meet the following requirements:

Minimum thickness (h) of 3", minimum width (w) of 12", minimum depth (d) of 12". Slabs shall be placed horizontal in a shingled effect according to the Riprap Specifications.

Concrete Sacks - shall meet the following requirements:

Minimum empty sack dimensions: 12" wide by 24" deep. Aggregate mixture shall be 4 parts sand/gravel and a minimum of 1 part Portland Cement. Openings of each sack shall be securely closed and placed opposite to potential water turbulence. Sacks shall be laid horizontal, in a shingled effect, according to the Riprap Specifications.

FOREST ROAD ACCESS

Road Maintenance Specifications

1. Prior to Acceptance of Contract or Acceptance on Timber Sale

A. Cuts and Fills

- (1) Maintain slope lines as constructed. Remove slides from the ditches and roadway. Replace fills to 1 ½:1 slopes with selected material or as directed. Remove overhanging material from cut slopes.
- (2) Material from slides or other sources requiring removal must not be deposited in streams or at locations where it will erode into streams or water courses.
- (3) Undesirable slide materials and debris must not be allowed to contaminate or mix with surface material.

B. Roadway Surfaces

- (1) Grade and shape road surface, turnouts and shoulder to original crown, inslope or outslope as directed to provide suitable traveled surface and surface water runoff in an even, unconcentrated manner.
- (2) Blading must not undercut backslopes at bottom of cut slopes.
- (3) Watering may be required to control dust and to retain fine surface rock.
- (4) Desirable surface material shall not be bladed off roadway.
- (5) Replace surface material lost or worn away.
- (6) Remove berms except as otherwise directed by the State.

C. Drainage

- (1) Keep ditches and drainage channels at outlets and inlets of culverts clear of obstructions.
- (2) Inspect and clean culverts at least monthly, with additional inspection during storms and periods of high runoff. This must be done even during periods of inactivity.
- (3) Place non erodable material or rock at drainage outfalls.
- (4) Keep silt bearing surface runoff from contaminating live streams.

D. Structures

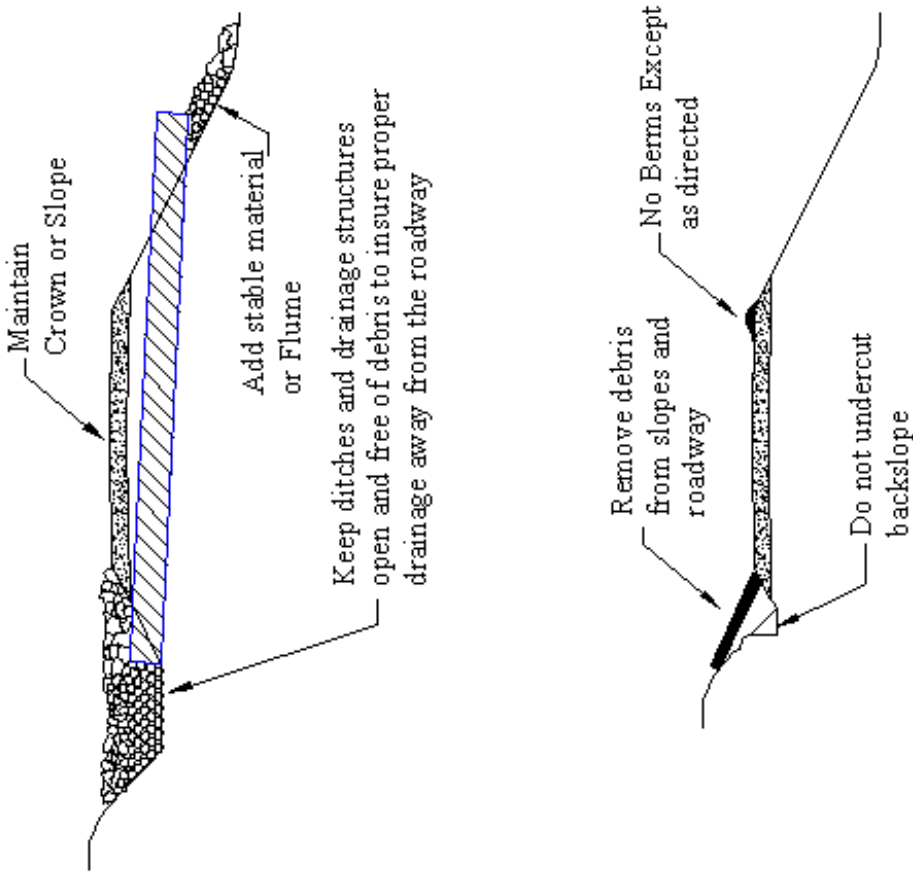
- (1) Repair bridges, culverts, cattle guards, fences and other road structures to conditions required by construction specifications.

E. Termination of Use, or End of Season

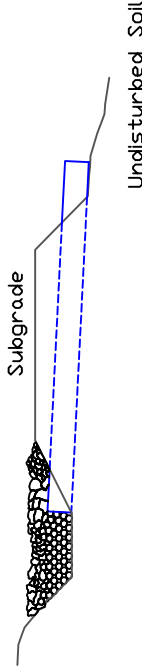
- (1) Do maintenance work to minimize damage from the elements such as blading to insure correct runoff, ditch and culvert clearing and water bars.

F. Debris

- (1) Remove fallen timber, limbs, stumps from slopes and roadway, ditchlines and culvert inlets.

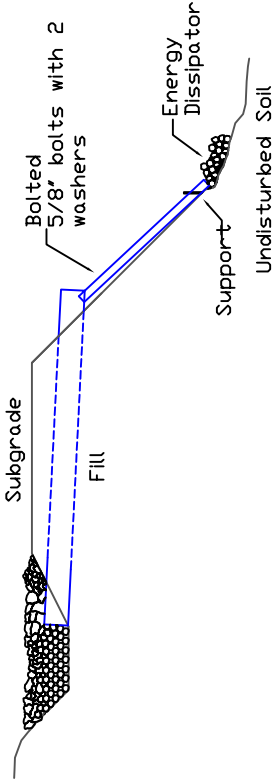


CULVERT AND DRAINAGE SPECIFICATIONS DETAIL - D1



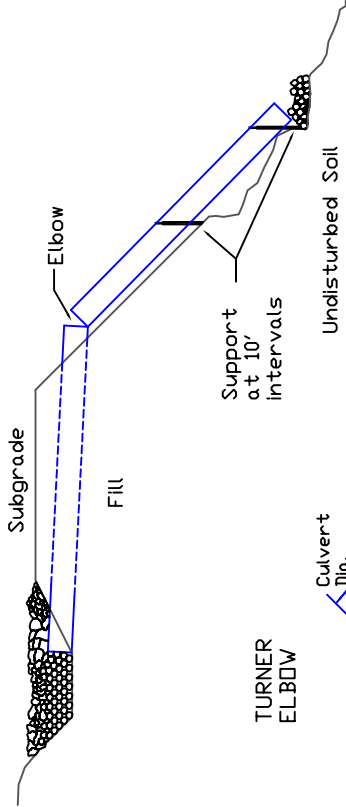
FLUME

Use where ground conditions are uniform, providing for stability of flume.

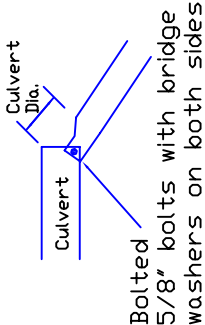


DOWNSPOUT

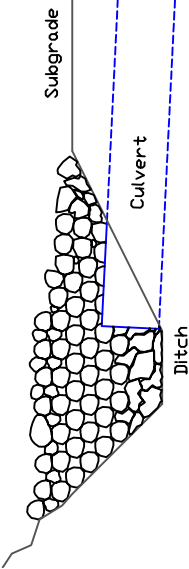
Use where ground conditions are irregular.



TURNER ELBOW

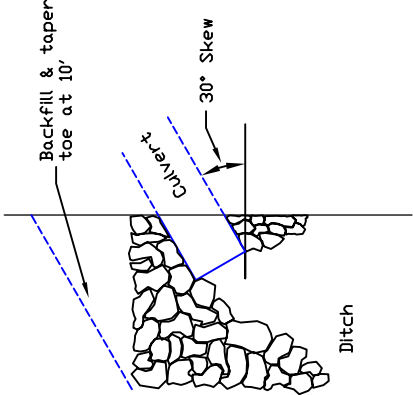


HEADWALLS



Headwall to be constructed of material that will resist erosion

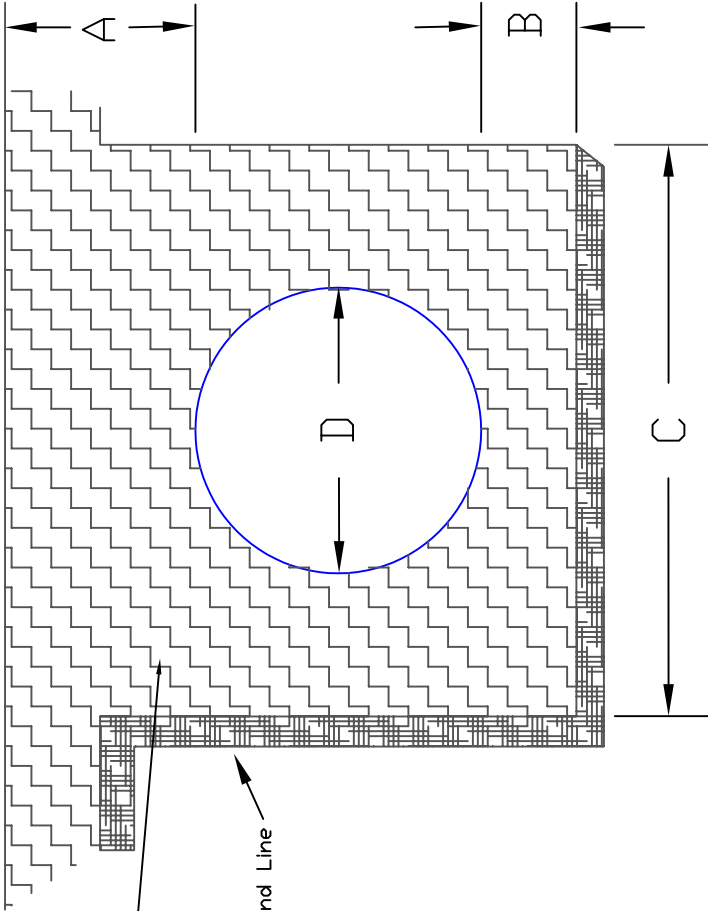
PLAN VIEW



CULVERT BACKFILL & BASE PREPARATION
(For Culverts Less Than 36")

Minimum Cover	Minimum Bed Depth	Min. Trench Width	Nominal Diameter
A	B	C	D
12"	6"	36"	18"
12"	6"	42"	24"
12"	6"	48"	30"
12"	6"	54"	36"

ROAD SURFACE

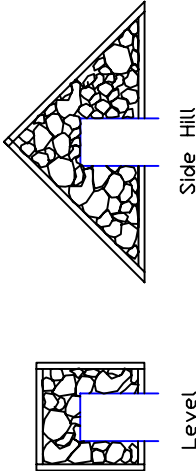


BEDDING MATERIAL:

Use granular material - 3" minus. Large rocks shall be replaced with suitable material. Materials of poor or non-uniform bearing capacity shall be removed and replaced with suitable fill.

DISSIPATOR SPEC'S
Size in Culvert Diameters

Area	2 X 2
Depth	1
Aggregate	1/3

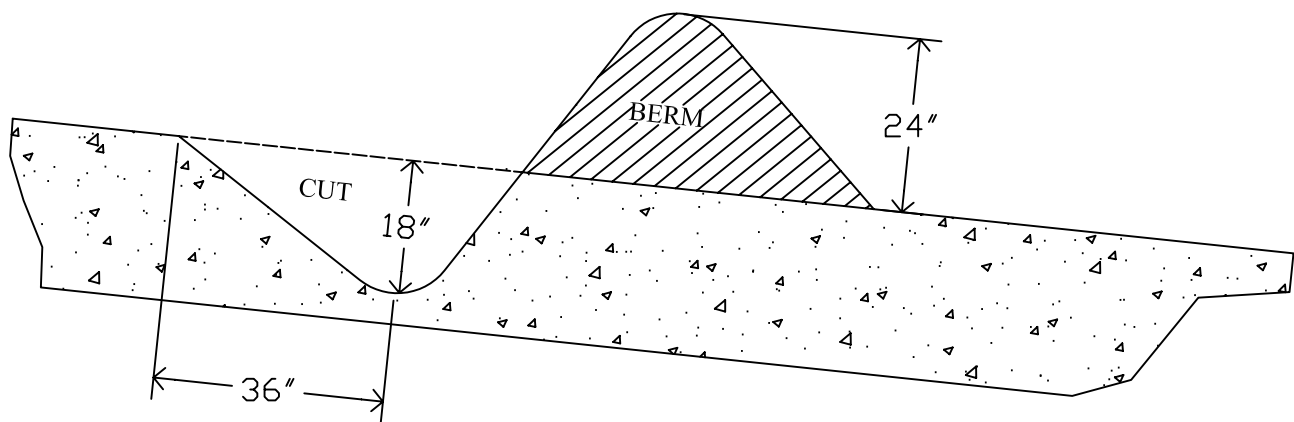


The minum cross grade from "B" to "E" is 1% greater than the original road grade.




Width	12'	14'	16'	ALL		
Dimension	G			H	I	J
Road Grade 6% and under	60	61	62	52	.8	0.3
8%	70	71	72	62	1.0	0.2
10%	80	81	82	72	1.1	0.1

Non-Driveable Water Bar Detail



Notes:

- 1. WATERBAR CONSTRUCTION FOR FOREST ROADS WITH NO TRAFFIC. SPECIFICATIONS ARE AVERAGE AND MAY BE ADJUSTED TO CONDITIONS.
- 2. TIE BERM INTO BANK. IF DITCH EXISTS, TIE CUT INTO DITCH.
- 3. CONSTRUCT CROSS DRAIN BERM APPROXIMATELY 24 IN. HIGH.
- 4. CUT WATERBAR A MINIMUM OF 18 IN.
- 5. ENSURE PROPER DRAINAGE AT OUTLET.
- 6. SKEW WATERBAR 30 DEGREES DOWNGRADE WITH ROAD CENTERLINE.

Non-Driveable Waterbar Detail		
 WASHINGTON STATE DEPARTMENT OF Natural Resources		
Revised:		1 OF 1

Northeast Region Colville, Washington		
Designed By:	Stash Slabinski	4/21/05
Drawn By:	Stash Slabinski	4/21/05

Name of Sale: Bull Goose

CONSTRUCTION CLASS
 NEW CONSTRUCTION = C
 RECONSTRUCTION = R
 PRE HAUL MAINTENANCE = P

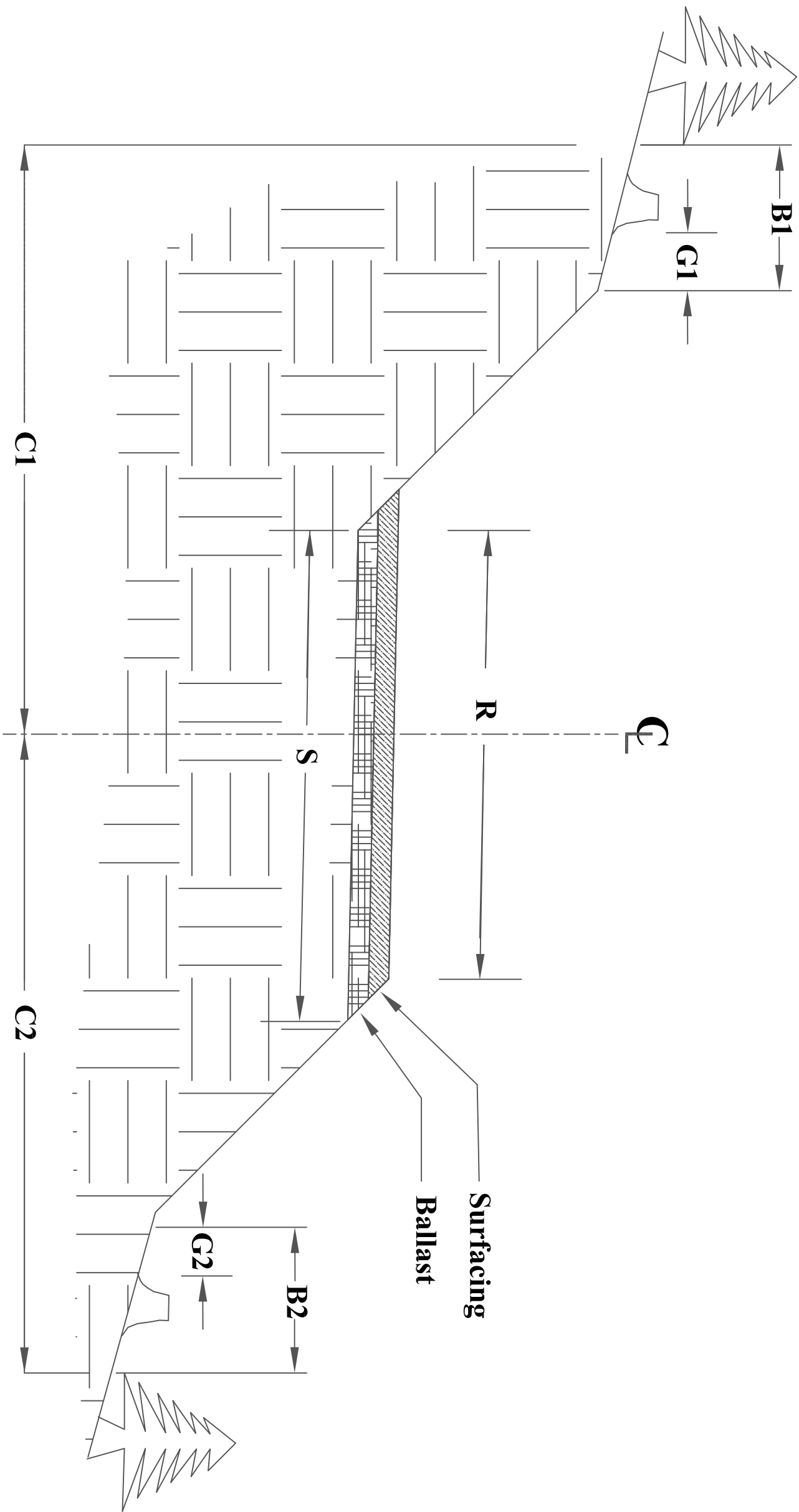
SECTION VIEW

**Turnout Detail
PLAN VIEW**

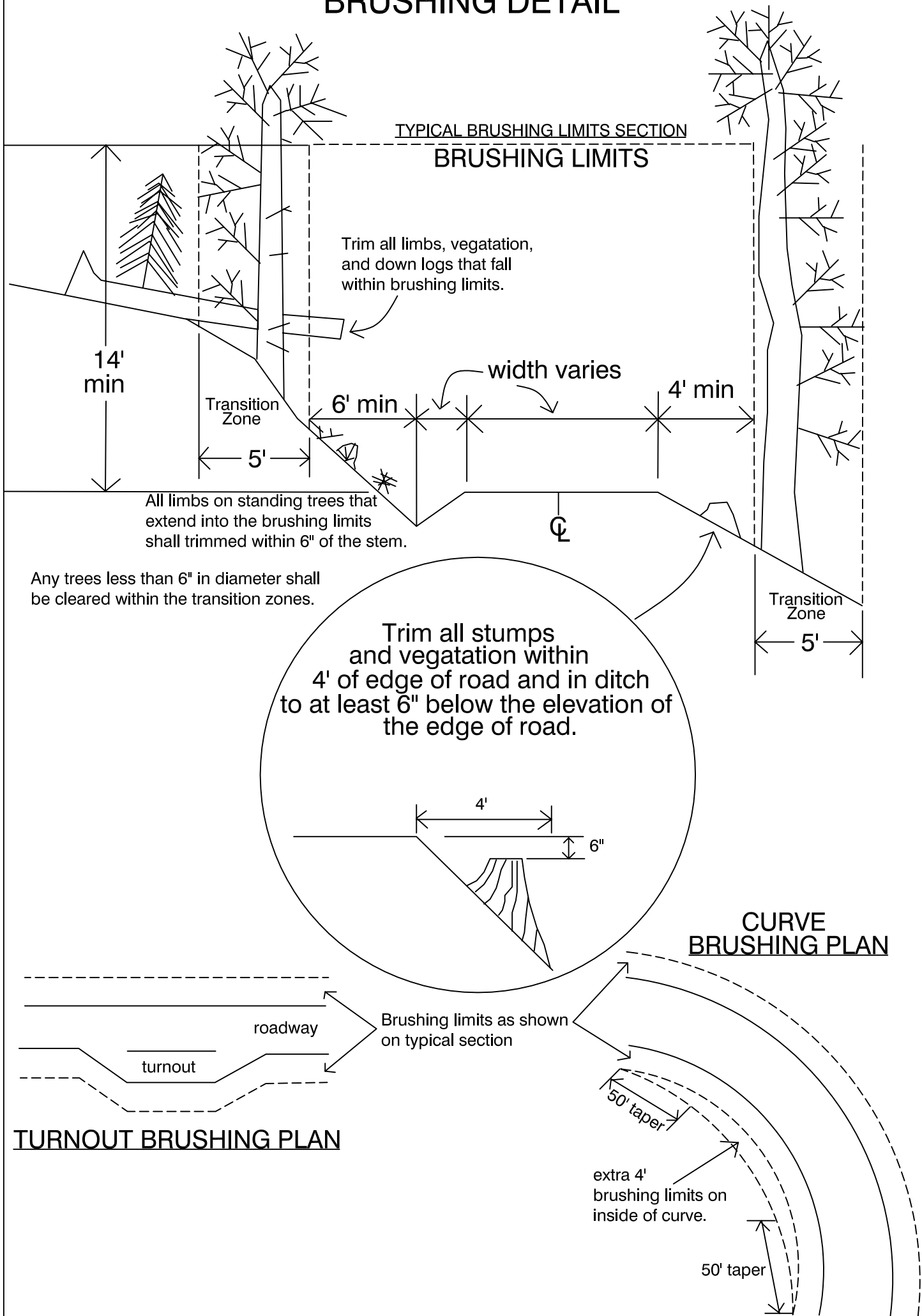
Date: 11-28-05

OUTSLOPED ROAD CROSS-SECTION

(not to scale)



BRUSHING DETAIL



1. All vegetation within the brushing limits shall be cut to within 8" of the ground, unless otherwise directed by the contract administrator.
2. All brush, trees, limbs, etc. shall be removed from the road surface and ditchline.
3. All debris that may roll or migrate into the ditchline shall be removed.